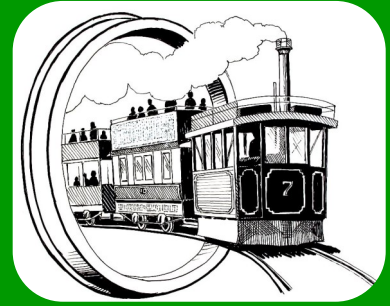


# Tracts.



May 2024

The Newsletter of the Tramway Historical Society Incorporated.



*Monthly Newsletter of the THS*

*Contributions welcome. Please send to [tracts@ferrymeadtramway.org.nz](mailto:tracts@ferrymeadtramway.org.nz)*

*All contributions must be in by the 23rd of the month. Tracts released for distribution on 1st of following month.*

# Tracts May 2024

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**AGM date.** 4th week of August 2024

**Heritage Tramway Trust.**

**Chairman:** Dave Sanders

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**Secretary:** Callum Brieske.

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**Treasurer:** Alan Hinman

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**Board Members:** Graeme Belworthy, Stephen Taylor, Nick Allen

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Cover Photo:

THS restored and owned trams on lease to Christchurch Tramway Ltd.

Invercargill Birney 15 and CTB Hills car 24 in Cathedral Square one sunny morning in April.

Photo by the Editor.

## Contributions to Tracts.

Many thanks to all that have contributed to this edition of tracts.

If there is a society related topic or old CTB memories or other similar articles you would like to put in the next edition.

Let me know.

Cheers Alex.

## Presidents Piece.

Jonathan Day Reports.

### Cranmer

Work in and around Cranmer has been progressing well with the theatrette having its building wrap installed and the new barge boards and gutter the next item to be completed within the next few weeks (weather permitting). Once that work is complete we will start on the internal fit out of the room.

Work on the storage area inside has been due to the assistance of the HTT team while they have had some downtime.

If anyone is available to help with any of the work in Cranmer please contact Stephen or Myself.

### Around the Trambarns:

We have been carrying out quite a bit of tram maintenance over the last month due to extra running over the school holidays, this is normally carried out by a small team on Monday nights from approximately 7:30pm until 9pm. If you have some spare time touch base with me or feel free to pop in and lend a hand, we are always looking for extra help.

The HTT workshop team has been getting ready for the

start of the Christchurch Tramway winter maintenance program which should be kicking off over the next month.

### On The Buses:

Exciting things are happening at the bus end of the site with 620 and 538 both currently COFed and roadworthy, we are hoping 614 will join them in the next few weeks after an issue with the accelerator cable was found, a new one has been sourced and will be fitted in the coming weeks.

Once 614 is roadworthy we will begin advertising for charter work, initially aimed at education trips to ferrymead and hopefully then expanding to include all aspects of charter work.

Watch this space for the launch of a special charter website and advertising, hopefully ready for the next issue.

### Future planning of the society;

The society has held its annual Goals and Projects meeting to go over the priorities for the society. The management committee will discuss the outcome of the meeting at the next meeting which will help determine the direction of the society over the next 12 Months.

Once reviewed at the May committee meeting the list will be made available on our website and a link will be sent around the society members to review.

## Membership Secretary update.

Callum Brieske Writes.

For those of you that have paid your subs you may have noticed that you are yet to receive your 2024 membership cards, and for those who haven't yet paid their 2024 subs, this should serve as a reminder.

Members of management committee are currently working towards modernising our record keeping, primarily to help us better comply with our regulatory requirements, but also to give us better insight into how we are operating as a society.

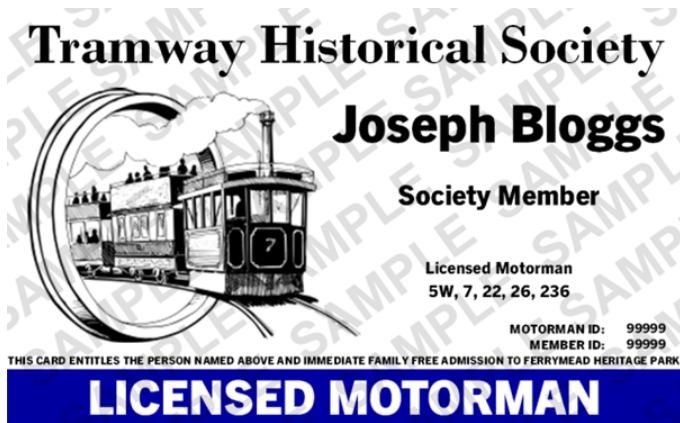
One aspect of this is a centralisation of membership, training, and licensing records. Unfortunately, this work has taken longer than anticipated, delaying the issuance of new membership cards. This has been compounded by the fact that this year we are issuing an updated membership card.

The new membership cards are intended to serve as a combined membership card, ID card, name badge, and (at a future date) door access card. We are working towards upgrading some of our building entrances to

RFID door locks, and your new membership card will in future allow you to access society buildings. In the meantime, those with keys can continue to use those as usual.

Pictured below is a sample of what your new membership cards will look like. It is worth noting that these cards don't have an expiration date, as they can be electronically expired. Membership privileges will only be conferred to current financial members.

These new cards will start going out next week, so keep an eye out for them in the coming week or two.





# Heritage Tramway Trust.

## Dave Sanders Reports

This has been a busy but muddily month as we get ready for #1888 to arrive from the city on the 1st May.

There is both some insurance repair work to be completed as well as some additional work required by Christchurch Tramway Limited.

Tram #1888 is one of the two trams used in the city that are not THS trams, the other being #411, the Restaurant Tram.

Having said that, it is an R Class built in Sydney in 1934 for the New South Wales Government Tramway. It is a 48 seat tram and is one of 195 built and is owned by the Sydney Tramway Museum.

#1888 arrived in Christchurch as #1808 and was renumbered to #1888 which is what we will refer to it as.

We have been notified of two accidents involving our trams in the last month. The first was to tram #178 which was involved in an accident when a member of the public drove into the side of the tram while exiting the Arts Centre.

There were no injuries, however there was substantial damage to the PC5 Controller under the side of the tram. Which is currently in TBI for insurance review prior to remedial work being undertaken.

The second was to tram #244 which was caught by a construction site gate. Damage was superficial with the HTT boys repairing a broken step.

Along with other general workshop work, our staff have completed:

- Work on the roof of Trailer #18,
- Work on the advertisement boards for CTL.

Ongoing work has included:

- Warren has working with Don McAra on Cable Car #103, and

A start has been made on work in the storage area in the east end of the Cranmer building. During April, work has included:

- Removal of some of the old mezzanine area,
- Installation of two pallet racks,
- Clearance and tidy up of the entrance to the storage area and storage racks for some of the screw containers and other small fixings.

Leighton has been working on another tool storage rack for the Morticer tooling, and the Woodworking Shop has had quite a bit of attention to the electrical services (thanks very much Murray and your team). The machinery plugs are largely now up to standard and the overhead fluorescent's have been

changed out with some LED tubes.

The new Dust Extractor is working with some piping issues yet to be resolved. The fire sprinkler system has been extended to the new lean to.

Pest control – The City Council has placed some possum bait traps outside Finally, This was a health issue raised at a recent Tailgate Safety discussion by our HTT staff concerning vermin control. We again have a serious issue with Opossum's. Our internal trapping of these beasts has not been effective. It is clear that these animals are coming into the Trambarn for warmth and comfort, not feeding .

After receiving an amazingly high quote our Workshop Manager has approached the City Council Ranger, who has agreed to place several external traps, with the hope that we can trap of furry squatters as they forage for food .

If any of our members have other ideas that may be successful, please contact and discuss with Graeme directly.

As always, contact me directly as you like on 021 423 763. I am generally down there on Wednesdays and Saturdays.

Ngā mihi nui



*R class 1888(1808) at Ferrymead in December 2017. During a test run before being moved to CTL.*



*178 with the car wedged between the PC5 controller and a traction pole at stop 12.*



## NSWGT R 1888 (1808) on the move.

Editor writes.

On a Drizzly morning on Wednesday 1st May, Ex Sydney tram 1808 was moved by road from Cathedral Square to Ferrymead, 8km away.

The Tram was lifted onto the transporter with the aid of 2 x 12ton forklifts.

This all went without issue and now the tram is safely placed in Tram Barn 1 at Ferrymead for some bodywork repairs.

It will return to CTL metals in early July.



## Standard Trailer 126 Clean up.

Ken Henderson reports.

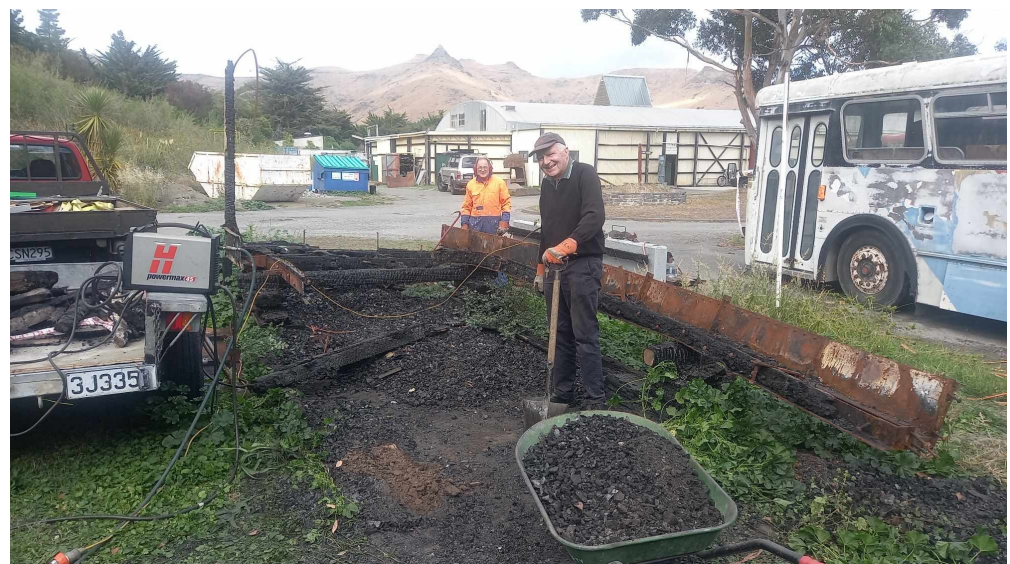
A few members turned out to help with the clean up of Standard trailer 126.

Stephen Taylor, Ken Henderson, Alan Hinman, Callum Brieske and Peanut set to cutting up the steel chassis remains and placing them in the metal scrap bin, and the charred wood on a trailer and taking it to the dump.

Then the area was raked over to clear away any bits so the grass can be sown again.

Many thanks to those who turned out to help.

Photos by Alan Hinman.





# Cramner Building Progress.

Stephen Taylor Reports.

There has been further good progress on the proposed meeting/presentation room on the south side of the west end of the building. In recent weeks we have (almost) finished off adding additional dwangs and wall framing to allow the interior walls to be re-clad in gib. We are also intending to put a gib ceiling in under the roof beams.

We have also applied a layer of building wrap to the exterior of the room – as part of making the building more weatherproof. This involved removing all the exterior corrugated iron, applying building wrap to the exterior and then replacing the building wrap. It actually noticeably lightens up the interior of the room. We have also added a layer of building paper under the edge of the roof over this area in an attempt to see fix the remainder of the roof leaks. Next steps are to finish putting barge boards and guttering on the exterior of this part of the building.

Once we have confirmed the exterior is fully weatherproof (we recently discovered some small niggling roof leaks) – and before the interior lining and insulation is added, we need to add interior wiring, audio-visual cabling, and plumbing for the heat pump to be installed in this room.

I have included a couple of photos of the interior showing the visual difference the building wrap has already made.

In addition to the above work, the HTT team has spent time in recent weeks in the east end of Cranmer building helping remove a large part of the old mezzanine floor and a start made to erect pallet racking for storage to help make this area more useful and tidy as a storage area. This will also allow some of the storage areas in Tram Barn 1 to be better utilised with a lot of surplus tram parts relocated to this pallet racking in Cranmer and make things easier to find in the future. This will not be a quick activity to fully complete and we expect to “chip away” at it as there is still a lot of further work to be done but it is good to see a start has been made.



*Top 3 photos:  
Showing the progress of the exterior cladding with the building wrap being installed and then the iron going back on finally as the sun sets. Photos by Alan Hinman.*



These photos are of the interior, it is noticeably lighter inside now with the wrap fitted : Photos by Stephen Taylor.



## Cable Car 103 Restoration.

Don McAra Reports.

Warren Crowe continues to make great progress completing the eight cabin doors for 103. I am now priming and putting on a couple of coats of undercoat on all four outer cabin walls. The insides of both inner and outer cabin walls are painted brick red just to preserve them as they are not seen.

Warren is fitting the brass brackets from which the doors are suspended. Allowance has been made for the doors to be shortened slightly when we actually hang them from the steel strips which are to be fastened behind the tops of the outer cabin walls.

Don Clark has almost finished the door opening mechanisms including the rails on which the door hangers roll to and fro.

Henry Deer has been contacting his pattern making friend Bill with drawings I did of the 16 cabin door handles, and there are a lot of small brass fittings with little wheels which enable the doors to smoothly open and shut to be made. Finally Graeme Richardson showed us how to make a pattern for a door step strip which also guides the doors at the bottom. The Mornington cars had much more complicated double acting doors than those in



Roslyn gripcars which were built by a different Dunedin company.

**The photo shows Henry standing by a pile of both outer and inner cabin walls. Standing on their ends further back are bits of cabin seats and side seating which are patterns for all the seats.**

We are lucky to have them.

## Did You Know... By Ken Henderson.

The airport at Napier had an unusual terminal for a number of years being an old tram body! A friend visiting from Napier recently sent me a photo from the current terminal.

Wikipedia explains the situation as follows: The previous airport in Napier was the Embankment Aerodrome, which was started in 1932 and located near the Napier end of the Napier–Westshore road and rail embankment. The Napier

Airport Board was formed in 1935 and the borough of Napier became owner of the aerodrome. While work was being done to bring it up to Civil Aviation Authority (CAA) standards, an alternative airfield was needed, so one was created near the shipping beacons between Westshore and Bay View. It was then realised that the Beacons

Aerodrome had more space for further development, so the airport board made it Napier's main airport. It was chosen as Hawke's Bay's main airport in 1944.

Napier only had 9 trams working on 3ft 6ins before the earthquake of 1931 wrecked the city. The fleet included 5

trams imported from England, 2 trams built by Boon & Co and 4 ex Auckland tram bodies of which #63 was never commissioned and is the one featured in the photo (ref End of the Penny Section, Graham Stewart). Six years after the earthquake 8 of the tram bodies were bought by the municipal motor camp.



## Drivers Tip. David Jones Reports ( Tips?)

A tip this time from Peanut. To reduce de-wirements on No 1, Peanut suggests that when securing the trolley rope, you pass the rope through the opening closest to the handrail. This reduces the length of the rope and minimises the risk of it hooking up on the destination box or air brake hose valve - as per attached photo. Makes good sense



## Newly weds on the tram!

David Jones Reports.

Our tram driver, Georgia, is now Mrs Roman and is seen here, driving No.1, on her wedding day with her husband Michael. Georgia and Michael were married in the church at Ferrymead and the reception was held in the lodge. A good time was enjoyed by all and we wish Georgia and Michael a long and happy life together.





## Exploring Dunedin's Cable Car Legacy.

Written by Brendon Carter.

I am excited to delve into my memories of the Mornington and Maryhill cable car lines. Born in 1950, I have vivid recollections of the final days of these iconic systems. Let's start with the Maryhill line. Passengers would traverse through the Mornington cable car shed to embark on their journey. As they stepped aboard the cable car, they entered the skilled domain of the gripman, ready for an unforgettable ride.

With a glance to ensure all passengers were aboard, the gripman would then seize the rope with precision. A series of grips and a mighty heave would propel the cable car over the edge, descending on the steep grade of 1-4 to 1-3 1/2, earning the Maryhill line its reputation as the steepest cable car system globally.

Yet, as the cable car descended towards Haig Street, caution enveloped both motorists and pedestrians, for the cable car's entry into Glenpark Avenue demanded utmost vigilance. Amidst managing fares and maintaining a clear view of the track ahead, the gripman skillfully navigated the descent. Passengers, urged to remain seated in the front cabin, embarked on a journey aboard one of two cable cars operating on the Maryhill route, numbered 105 and 106 respectively. Today, No. 105 finds its home at the Seashore Trolley Museum in Kennebunkport, Maine, USA, while No. 106 resides in Dunedin's Settler's Museum, albeit without its gripper, still traversing the Maryhill line.

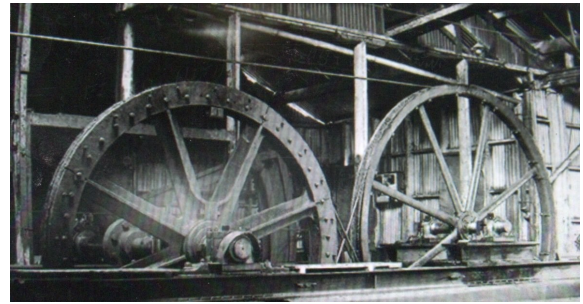
Above Neidpath Road, a unique loop awaited Maryhill's cable cars, marking the last of its kind outside of the United States. Upon arrival at Maryhill, the gripman would signal the engineer to reverse the rope, preparing for the return journey. Back at Mornington, the gripman would release the rope and engage the brake on a short section of track, signaling the end of the memorable ride. Observing the cable car's arrival from the alleyway between the M.M.M. butchery and the Mornington cable car sheds was truly an eerie experience, etched in the memories of those who witnessed it.

Despite often referred to as an extension, the Maryhill line stood as a separate entity, spanning half a mile in length. Traversing under the roadway, the rope and sheaves ensured a smooth journey for passengers. However, the line faced challenges, including the need for a replacement rope and the temporary use of motor buses, which inflicted damage upon the cable car tracks along Glen Park Avenue. A New rope was fitted which only saw four days of operation.

In February of 1956, the council made the decision to close the Maryhill line, although the cable car tracks remained intact, serving as a silent testament to Dunedin's rich cable car legacy. Meanwhile, the rope found its final resting place at the state no.2 coal mine near Greymouth.

As we reminisce about Dunedin's cable car era, these memories serve as a testament to the ingenuity and resilience

of a bygone transportation era, leaving an indelible mark on the city's history.



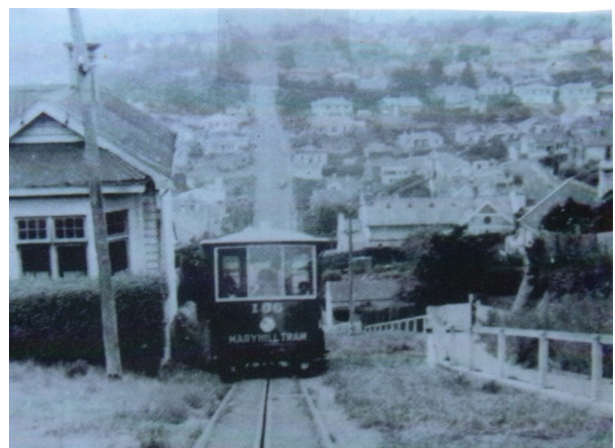
*A winding engine—Photographer unknown.*



*Maryhill 105 was a spare car. Photo— Evening Star.*



*Maryhill 106 on the descent - Photographer unknown.*



*Maryhill 106 near Elgin Rd. Photo —Martin Van Raalte.*



## Then and Now..

The Scene this month has changed quite dramatically, Taken in April 2010, as a few months after the top photo was taken was the first earthquake in 4th September 2010, Location is the corner of Colombo and High and Hereford Streets, during the construction of the new loop of the CTL system. Photo supplied by Stephen L Taylor.

Bottom Photo shows the same scene today with CTB Brill car 178 heading away from stop 9 on the CTL system.

